

# Pencoed Primary School, Planning Development Brief

## 1. Purpose of Report

- 1.1 For the Development Control Committee to consider the Pencoed Primary School Planning and Development Brief (attached as Appendix 1) and approve its content as a material consideration in the determination of any future planning application for the site.

## 2. Connection to Corporate Improvement Plan / Other Corporate Priorities

- 2.1 The approval of the Development Planning Design Brief will contribute to the Council's Corporate Improvement priorities of working together to develop the local economy and working together to make the best use of our resources.

## 3. Background

- 3.1 As part of Bridgend County Borough Council's (BCBC) ongoing School Modernisation Programme, Pencoed Primary School is set to be relocated (including the Heol Y Cyw Campus) to a new build school on the site of Pencoed Primary School playing fields at Penprysg Road with effect from the 1st April 2018 (Planning Application P/16/603/BCB refers). Accordingly, BCBC are seeking to dispose of the existing Pencoed Primary School site, which has been declared surplus to requirement.
- 3.2 The site does not currently benefit from an extant planning permission for redevelopment and it is not proposed that this will be secured prior to marketing the site for early disposal. **The site is however, allocated for residential development in the Bridgend Local Development Plan for approximately 10 units, LDP Policy COM2 (30) refers. However, it is anticipated that the site can accommodate between 20-30 residential units based on accepted dwellings per hectares standards.**
- 3.3 As part of the disposal process, and given that a planning permission is not being pursued prior to marketing, it is necessary to investigate any technical issues which may constrain the site's potential in order that a degree of certainty can be given to potential bidders on the site's suitability and capability for redevelopment. This requires consideration of a number of key issues including highways (both capacity and the ability to create any new vehicular access that may be required) and drainage. To inform prospective bidders and demonstrate the site's considerable potential, the Council has commissioned relevant consultants to investigate the technical issues associated with the site.

## **4.1 Current Situation**

4.1 The Planning Development Brief attached as Appendix 1 considers the context for the redevelopment of the site and puts forward a number of development parameters and design principles, which can be used to guide the sites' redevelopment. The purpose of the document is not to present detailed proposals, but to set a framework within which a high quality development can be delivered.

4.2 In summary the document comprises:

- The key delivery objectives - which sets out the Council's requirements for a high quality development that will maximise the potential of this site;
- The Planning Policy context against which the site will be assessed including national planning policy and the adopted Local Development Plan;
- Technical matters – including Transport Statement, Topography Study and Surface & Foul Water Drainage Strategy;
- Site and area analysis - which includes an overview of the site, its location and key characteristics within the urban area;
- The overarching design principles and parameters for the site including guidance on:
  - Accessibility and movement
  - Key frontages
  - Height, scale and massing
  - Residential amenity
  - Detailed design appearance and materials
  - Public realm and landscape
  - Drainage
  - Energy Efficiency and generation

## **5. Effect upon Policy Framework & Procedure Rules**

5.1 The Planning and Development Brief will represent a material consideration in the determination of future planning applications for the site.

## **6. Equality Impact Assessment.**

6.1 An Equalities Impact Assessment Screening has been undertaken and the proposed recommendations are unlikely to have an impact on equality issues.

## **7. Financial Implications**

- 7.1 There are no immediate or direct financial implications as a consequence of this report and accompanying Planning & Development Brief although it is envisaged that the document will encourage more developers to bid for the site.

## **8. Recommendations**

8.1 That the Development Control Committee:

- a) Approve the Pencoed Primary School Planning and Development Brief as a Development Control Guidance to be used as a material consideration in the determination of future planning applications for the site; and
- b) Delegate authority to the Group Manager of Development to make any necessary factual and/ or cartographical amendments to the document that may be required prior to formal publication.

**Mark Shephard**  
**Corporate Director Communities**

### **Contact Officer**

Richard Matthams  
Development Planning – Team Leader  
Telephone Number: 01656 643162, e-mail: [richard.matthams@bridgend.gov.uk](mailto:richard.matthams@bridgend.gov.uk)

### **Background documents**

1. Draft Pencoed Primary School Development Brief.

This page is intentionally left blank

# Pencoed Primary School

## Appendix 1



© Google Earth

Planning Development Brief

January 2017

# Contents

<b>Contents</b>	
Introduction, Vision & Purpose of Brief	1
Site Description and Wider Context	2-5
Planning Policy Context	6-7
Local Planning Policy	8-10
Technical Matters	11
Site and Area Analysis	12-17
Design Principles	18-24
Planning Application Requirements	25
Planning Obligations	26-27
Conclusion	28

# Introduction, Vision & Purpose of Brief

## Introduction

As part of Bridgend County Borough Council's ongoing School Modernisation Programme, Pencoed Primary School is set to be relocated (along with the Heol y Cyw campus), to a new-build school on the site of Pencoed Primary School playing fields at Penprysg Road, with effect from 1 April 2018 (Planning application P/16/603/BCB refers). Accordingly, Bridgend County Borough Council are seeking to dispose of the existing Primary School site, which will be declared surplus to requirements.

## Purpose of Brief

The purpose of this Development Planning Design Brief is to clarify the relevant planning policy context of the site; to provide a context appraisal of the redevelopment opportunities presented by the site together with setting out general development parameters for any future development schemes and to promote the site for alternative use. The Brief is intended to assist all parties that may have an interest in the sale - both present and in the future.

In preparing this Brief relevant Council departments have been consulted. This approach has allowed the principles and parameters for the site to be explored and tested with officers in terms of realism and deliverability, prior to it being endorsed by the Council's Development Control Committee.

## The key delivery objectives are to:-

- Deliver a high quality residential development;
- Provide innovation in design and sustainability;
- Provide a high quality public realm;
- Provide safe and attractive pedestrian links; and
- Minimise the impact of vehicular traffic within the development promoting walking and cycling in accordance with the Active Travel (Wales) Act 2013.

# Site Description and Wider Context

## Site Description

Pencoed is located to the west of the A473 which forms a north-south bypass to the town. The A473 links Bridgend to Talbot Green and has a junction with the M4 (junction 35), one mile south of the town. Access points to the town include Coychurch Road, Hendre Road and Penybont Road.

There are a number of regional services (Arriva Trains) including the Maesteg services, that stop in Pencoed (hourly) Monday to Saturday. With regard to bus services the town is served by the hourly service (no. 244 Bridgend-Talbot Green-Pontypridd) and a number of more local services (nos. 44, 45, 46 & 62).

The site measures approximately 1.8 acres (0.72 hectares) and is located at the northern end of Pencoed, approximately 4.5km to the north-east of Bridgend town centre. The site is situated in an established residential area and is bounded by residential development to the south, east and west. To the north of the site lies St Davids Church, which is a Grade II Listed Building.

The existing school complex comprises a group of 1 and 2 storey educational buildings dating from

the 1900s to more recent additions constructed in the late 1990s to 2000s.

The site does not currently benefit from an extant planning permission for redevelopment and it is not proposed that this will be secured prior to marketing the site for early disposal. The site is however, allocated for residential development in the Bridgend Local Development Plan for approximately 10 units, LDP Policy COM2(30) refers. However, it is anticipated that the site can accommodate between 20-30 units.

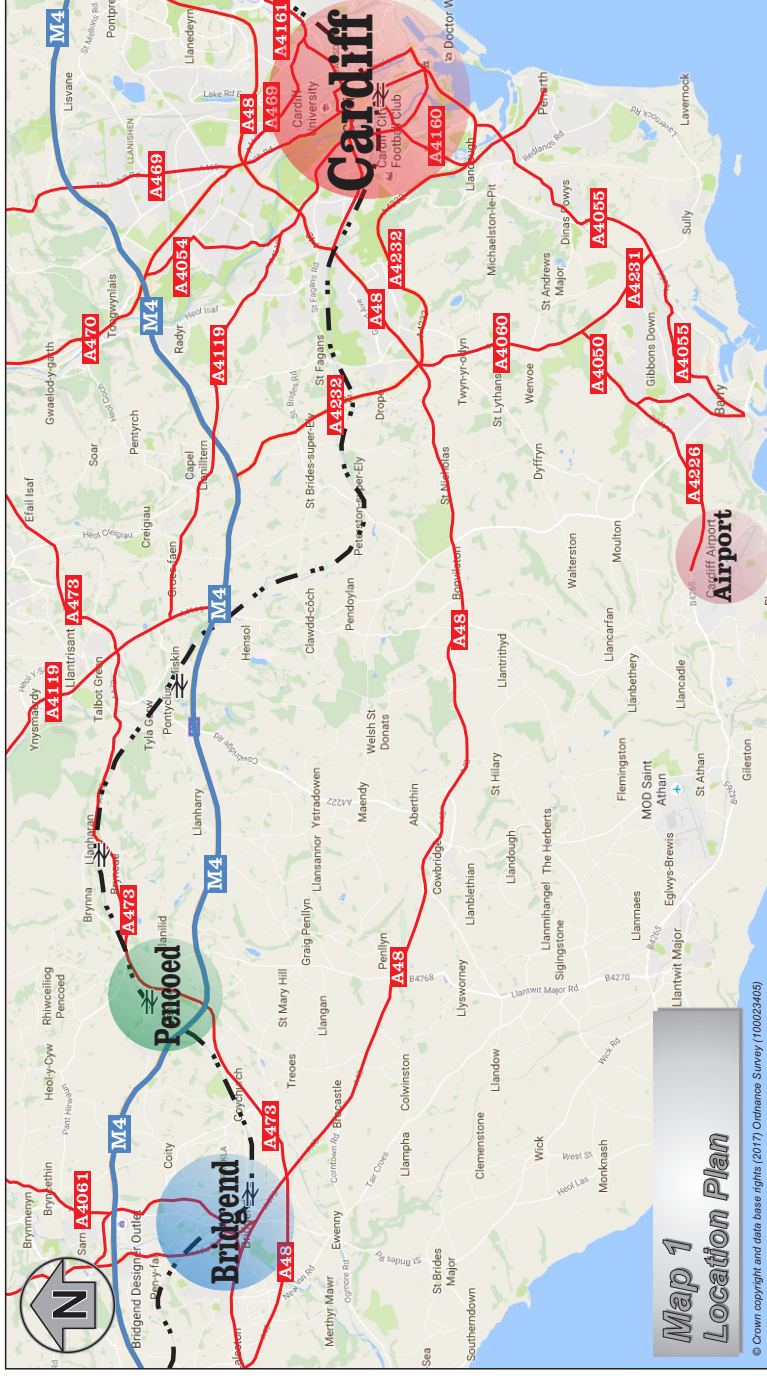
As part of the disposal process and given that a planning permission is not being pursued prior to marketing, it is necessary to investigate any technical issues which may constrain the site's potential in order that a degree of certainty can be given to potential bidders on the site's suitability and capability for redevelopment. This requires consideration of a number of key issues including highways (both capacity and the ability to create new vehicular access that may be required), drainage, the existing infrastructure and ground conditions. To inform prospective bidders and demonstrate the site's considerable potential, the



Council has commissioned relevant consultations to investigate the technical issues associated with the site.



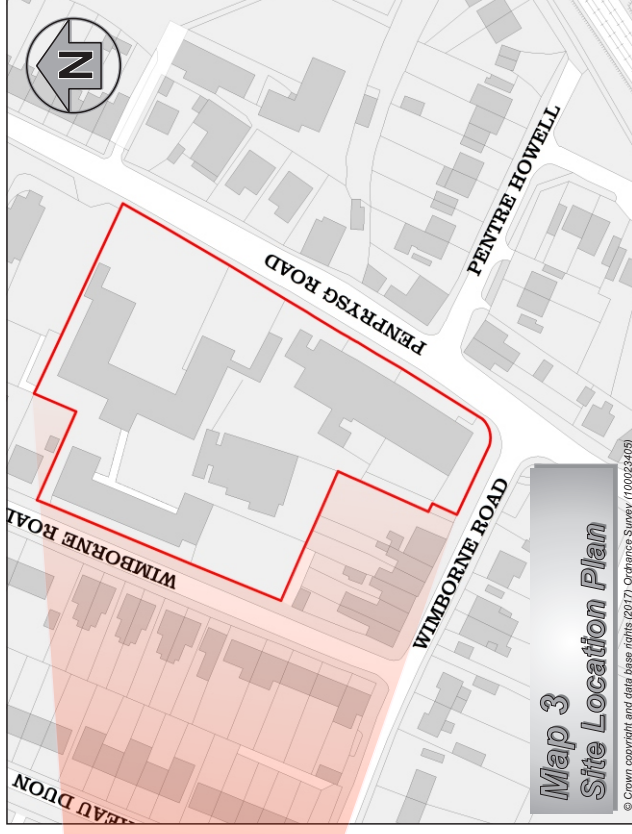
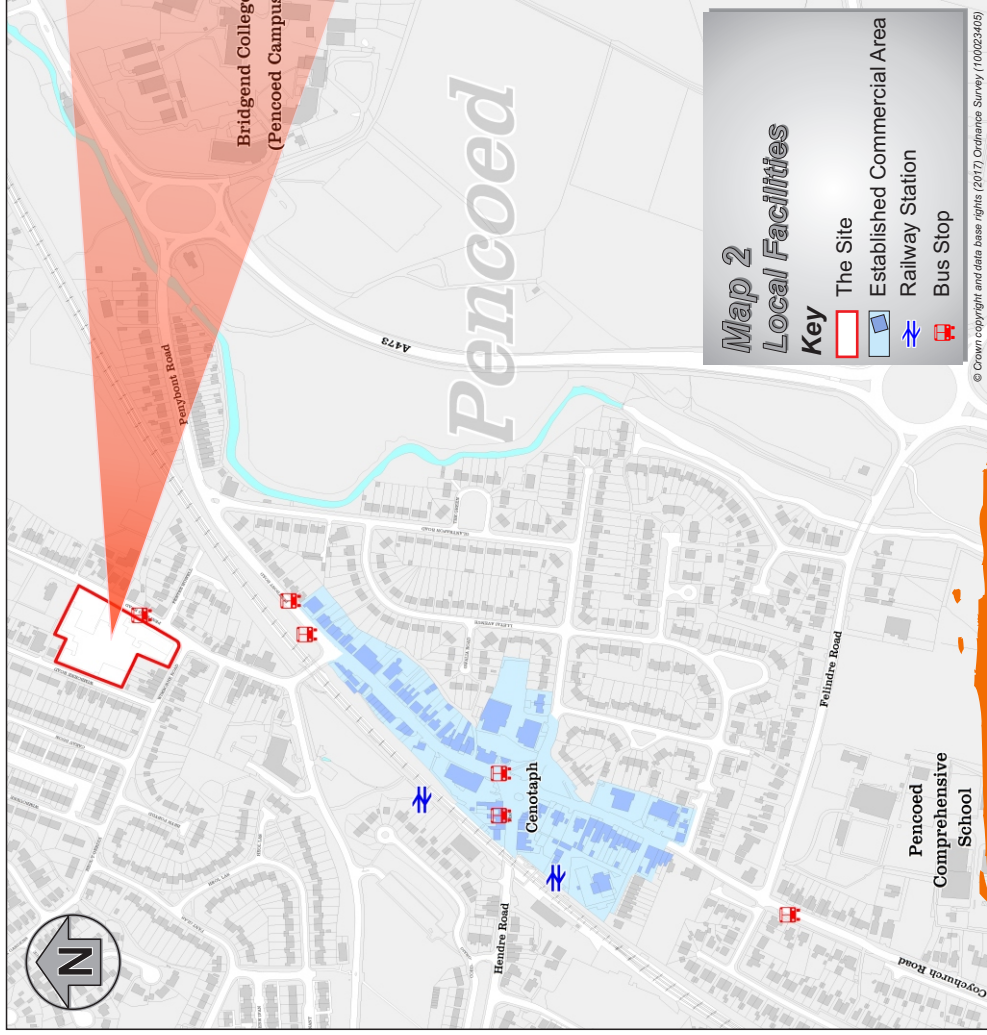
# Site Description and Wider Context



# Site Description and Wider Context



# Site Description and Wider Context



The existing vehicular access to the site is achieved via a single point off Wimborne Road. This access point is currently approximately 3.5 metres wide. Three pedestrian access points are provided along Penprysg Road and Wimborne Road. Penprysg Road is restricted to 30mph. In terms of topography, the site and surrounds are generally flat. The site contains some boundary trees, the most mature of which are located on the site's eastern boundary along Penprysg Road.

# Planning Policy Context

## Planning Policy Context

This section summarises the relevant planning policy context against which any redevelopment proposal for the site will be considered. Bridgend County Borough Council formally adopted the Bridgend Local Development Plan (LDP) in September 2013, and it therefore represents the Development Plan for the area. The document covers the plan period 2006-2021 and it sets out the objectives for the use of land in the area and the policies to implement them. The LDP is also supported by Supplementary Planning Guidance (SPGs). National Planning Policy Guidance is provided within Planning Policy Wales and accompanying Technical Advice Notes (TANs) which are also referenced below.

## National Planning Policy

### Planning Policy Wales 8th Edition (2016)

Planning Policy Wales (PPW) sets out the land use planning policies of the Welsh Government which seeks to achieve sustainable development. A summary of the relevant key policies, themes and drivers is provided below:

- To deliver resource and energy efficiency development that is climate change resilient;
- To encourage the reuse of previously developed land and buildings;
- To ensure the conservation of the historic environment and cultural heritage, acknowledging and fostering local diversity;
- To promote quality, lasting, environmentally-sound and flexible employment opportunities;
- To respect and encourage diversity in the local economy, promote established town, district, local and village centres as the most appropriate locations for retailing, leisure and other complementary functions;
- To promote high quality design;
- To ensure that all local communities - both urban and rural - have sufficient good quality housing for their needs; and
- To encourage the reduction in car borne trips by locating development close to existing development and encouraging trips by public transport, walking or cycling.



# Planning Policy Context

This confirms that National Planning Policy supports the provision of new sustainable development in defined settlements such as Pencoed and that new development should respect, conserve and enhance the historic environment.

The guidance within PPW is supplemented by a range of other Technical Advice Notes (TANs) that will also require consideration as future proposals for the site are developed. However the following are particularly relevant:

- TAN 12: Design (2016);
- TAN 18: Transport; and
- Active Travel (Wales) Act 2013.



## Bridgend Local Development Plan (LDP)

The Bridgend Local Development Plan (LDP) was adopted by Bridgend Council on 18th September 2013. LDP Policy PLA1 recognises the importance of Pencoed as a main settlement that offers a strong employment function with an existing concentration of business and a good variety of retailing and community services that meet the needs of the settlement and surrounding area.

### **Policy PLA6: Development West of the Railway Line, Pencoed**

This Policy seeks to prevent a net increase in vehicular traffic movement in Pencoed. This policy does not apply to the site.

### **Strategic Policy SP2: Design and Sustainable Place Making**

Strategic Policy SP2 introduces 15 sustainable place making criteria which will be applied to all development proposals across the County Borough. This policy represents the starting point for the assessment of all planning applications which are received by the local planning authority. Policy SP2 demands a high quality of design incorporating equality of access in all development proposals. Design and Access Statements will be used within the development control process and should contain information relating to each of the 15 criteria (where appropriate) to ensure this policy is implemented effectively.

### **Strategic Policy SP3: Strategic Transport Planning Principles**

Strategic Policy SP3 states that all development proposals should promote safe, sustainable and healthy forms of transport through good design, enhanced walking and cycling provision, and improved public transport provision.

### **Strategic Policy SP5: Conservation of the Built and Historic Environment**

Strategic Policy SP5 seeks to ensure that new development should conserve, preserve, or enhance the built and historic environment of the County Borough and its setting. In particular, development proposals will only be permitted where it can be demonstrated that they will not have a significant adverse impact upon (inter alia) Listed Buildings and their settings.



# Local Planning Policy

## **Policy COM2: Residential Allocations outside the Strategic Regeneration Growth Areas**

Policy COM2 identifies sites that are allocated for residential development outside of the Strategic Regeneration Growth Area.

## **Policy COM4: Residential Development**

Policy COM4 states that on sites larger than 0.15 hectares, new residential development will be built at a minimum density of 35 dwellings per hectare, unless local constraints or characteristics, or a lack of housing choice, dictate otherwise.

## **Policy COM5: Affordable Housing**

Policy COM5 confirms that where a need is demonstrated affordable housing contributions will be sought on development sites capable of accommodating 5 or more units, or exceeding 0.15 hectares in size. In Pencoed, provision of 20% will be sought on all residential development sites.

## **Policy PLA11: Parking Standards**

Policy PLA11 requires all development to provide appropriate levels of parking in accordance with adopted car parking standards.

## **Policy ENV5: Green Infrastructure**

Policy ENV5 seeks to provide Green Infrastructure across the County Borough through the protection and enhancement of existing natural assets and the creation of new multi-functional areas of green space.

## **Policy ENV6: Nature Conservation**

Policy ENV6 seeks to ensure that, wherever possible, development proposals retain, enhance, conserve or restore natural features and where this is demonstrated not to be possible, proposals should mitigate for any detrimental impact.

## **Policy ENV7: Natural Resource Protection and Public Health**

Policy ENV7 requires development proposals to demonstrate that they would not result in an unacceptable risk of harm to health, biodiversity and/or local amenity.

## **Policy ENV15: Waste Management in New Development**

Policy ENV15 requires all development proposals to include provision for the proper design, location, storage and management of waste generated by the development.

## **Policy ENV17: Renewable Energy and Low/Zero Carbon Technology**

Policy ENV17 seeks to encourage all major development proposals to incorporate schemes which generate energy from renewable and low/zero carbon technologies. If viable, all proposals of 10 dwellings or more are expected to be capable of connecting to district supply networks for heat and energy. Energy assessments should accompany all major applications to demonstrate consideration of renewable and low/zero carbon technologies.

## **Policy COM11: Provision of Outdoor Recreation Facilities**

Policy COM11 seeks to ensure that a satisfactory level and standard of outdoor and children's playing space and public open space will be required for all new housing developments. Provision of open space, or financial contributions towards such provision, will be in line with the standards set out in the LDP.

## **Pencoed Regeneration Strategy**

Hyder Consulting was appointed by Bridgend County Borough Council (the Council) in partnership with Pencoed Town Council in December 2009 to produce a Regeneration Strategy and Action Plan for Pencoed which was adopted as Supplementary Planning Guidance in 2011. The document identifies 18 projects which can be seen in full by following the link:

[http://www.bridgend.gov.uk/media/138467/Pencoed\\_Regeneration\\_Strategy.pdf](http://www.bridgend.gov.uk/media/138467/Pencoed_Regeneration_Strategy.pdf)



## Technical Matters

The constraints and opportunities presented by the site and the way in which these are addressed, provide the key influences on the development potential. Understanding these issues allows for a coherent set of development and design parameters to be established which will facilitate the delivery of a high quality development scheme.

The technical investigations undertaken to inform this development brief are summarised in this document with detailed reports attached as appendices in the technical marketing pack. The technical work includes the following:

- Transport Statement;
- Topography Study; and
- Surface and Foul Water Drainage Strategy.

Vectos were appointed by Bridgend County Borough Council to provide a Transport Statement and Surface & Foul Water Drainage Strategy in relation to the re-development of Pencoed Primary School based on a development density of 30 dwellings.

The Transport Statement assesses the likely effect of providing 30 residential dwellings on this site and sets out how the proposed development can be accessed by all modes of transport. The document concludes that there are no transportation or highway reasons why Pencoed Primary School should not be re-developed for residential use. The Transport Statement is attached as Appendix 1.

Welsh Water have confirmed that capacity exists in their foul network for 30 residential units, without the need for any upgrading works. The Foul Water Drainage Strategy is attached as Appendix 2.

# Site and Area Analysis

## Movement and Connections (including vehicular access)

This section of the Brief reviews the issues and opportunities relating to the main urban design elements of the site and the surrounding area.

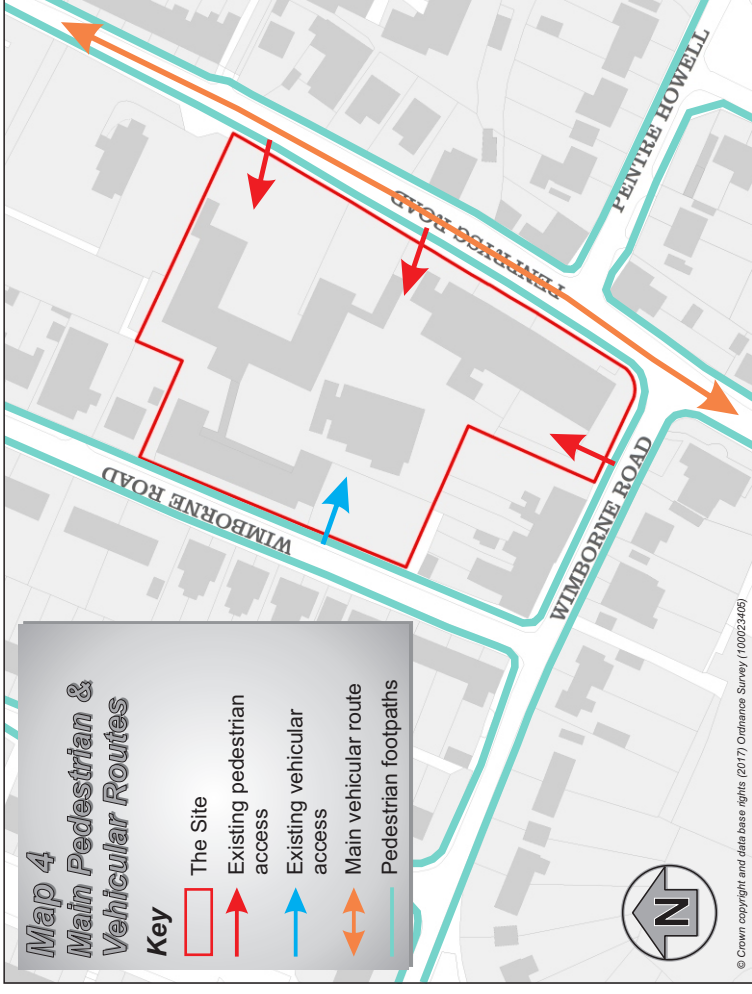
A Transport Statement has been undertaken (see Appendix 1). This did not identify any particular constraints to development at the site.

The site is located immediately adjacent to existing residential areas and is well-served by a range of transport means including public transport. Local pedestrian routes are provided as footways at the sides of the local highways which also benefit from street lighting. These provide connections to the surrounding shops, facilities and residential areas. The site is located within a prominent position with local accessible amenities within a 15 minute walk, as demonstrated by the table opposite.

Local Facility	Distance	Approximate Walking Time
Train Station	570m	7 mins
Bridgend College (Pencoed Campus)	866m	10 mins
Pencoed Comprehensive School	940m	11 mins
Public House	480m	6 mins
Post Office	309m	4 mins
Food Store	451m	5 mins
Bank	475m	5 mins
Medical Centre	720m	9 mins
Swimming Pool	900m	12 mins

# Site and Area Analysis

## Movement and connections



# Site and Area Analysis

## Existing land uses and activities

The site is located in an area characterised by a mix of uses including residential, commercial and community. The surrounding area contains medium density housing made up of a range of semi-detached, detached and terraced dwellings. Map 5 shows the existing land uses in the area.



# Site and Area Analysis

## Building heights

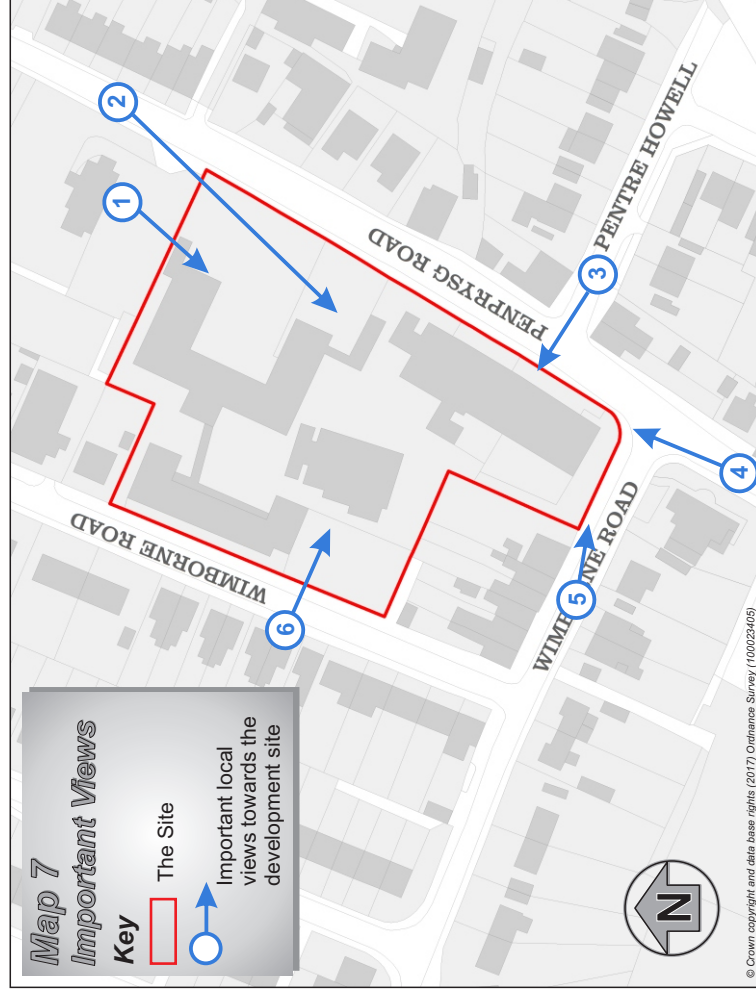
The scale of the surrounding development is shown on Map 6 below and predominantly consists of two storey buildings.



# Site and Area Analysis

## Important views

An important issue when considering the development of this site is the effect on existing views and vistas in the surrounding area. It is therefore important to identify each of these views as part of the design process.



### ① and ② (from the south)

Key views from Penprysg Road and St Davids Church looking south towards the site.

### ③ (from the east)

Key view from Pentre Howell looking eastwards towards the southern part of the site.

### ④ (from the north)

Key view from Penprysg Road looking towards the listed Church.

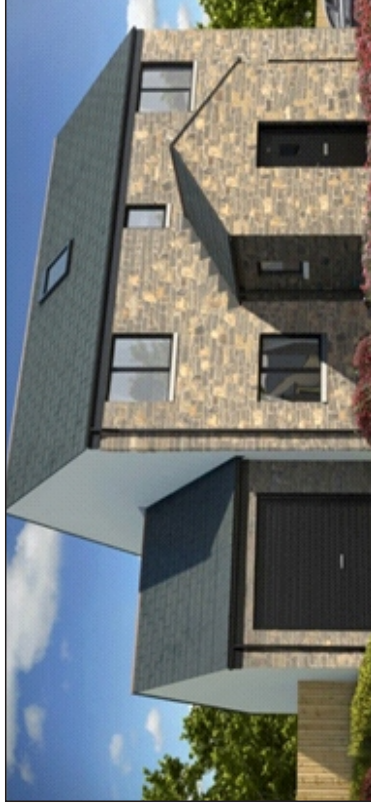
### ⑤ and ⑥ (from the west)

Key views from Wimbourne Road looking westwards towards the site.

# Site and Area Analysis

## Acceptable uses

It is anticipated that the site can accommodate a maximum of 30 residential units subject to meeting the requirements of this brief and Local Development Plan Policy SP2 and other relevant guidance such as SPG17 and TAN12.



# Design Principles

## Design Principles

The value of good design is recognised as being essential to the development and to the sustainable regeneration of Pencoed. A poor quality environment is both economically and socially unacceptable. Poor design is now proven to be costly. New development will be required to successfully regenerate this important site and improve place perception and sense of place.

The Development Principles which are described on the following pages have been produced in order to help guide detailed design work and future planning applications. These Principles are formulated in order to clearly set out aspects of design which are considered important to the Council, whilst providing the future developer (and their design team) ample scope for interpretation and innovation.

## Accessibility & Movement

The Welsh Government supports a transport hierarchy in relation to new development that establishes priorities in such a way that, wherever possible, they are accessible in the first instance by walking and cycling, then by public transport and then finally by private motor vehicles. The Welsh Government expects that Design and Access Statements give consideration to accessing developments by modes other than private motor vehicles.

Planning Policy Wales (para 8.1.5) states that land use planning can help to achieve the Welsh Government's objectives for transport through:

- Reducing the need to travel, especially by private car, by locating development where there is good access by public transport, walking and cycling;
- Locating development near other related uses to encourage multi-purpose trips and reduce the length of journeys;
- Improving accessibility by walking, cycling and public transport;
- Ensuring that transport is accessible to all, taking into account the needs of disabled and other less mobile people;
- Promoting walking and cycling;
- Supporting the provision of high quality public transport;
- Supporting traffic management measures;
- Supporting necessary infrastructure improvements; and
- Ensuring that, as far as possible, transport infrastructure does not contribute to land take, urban sprawl or neighbourhood severance.



# Design Principles

## Promoting active travel

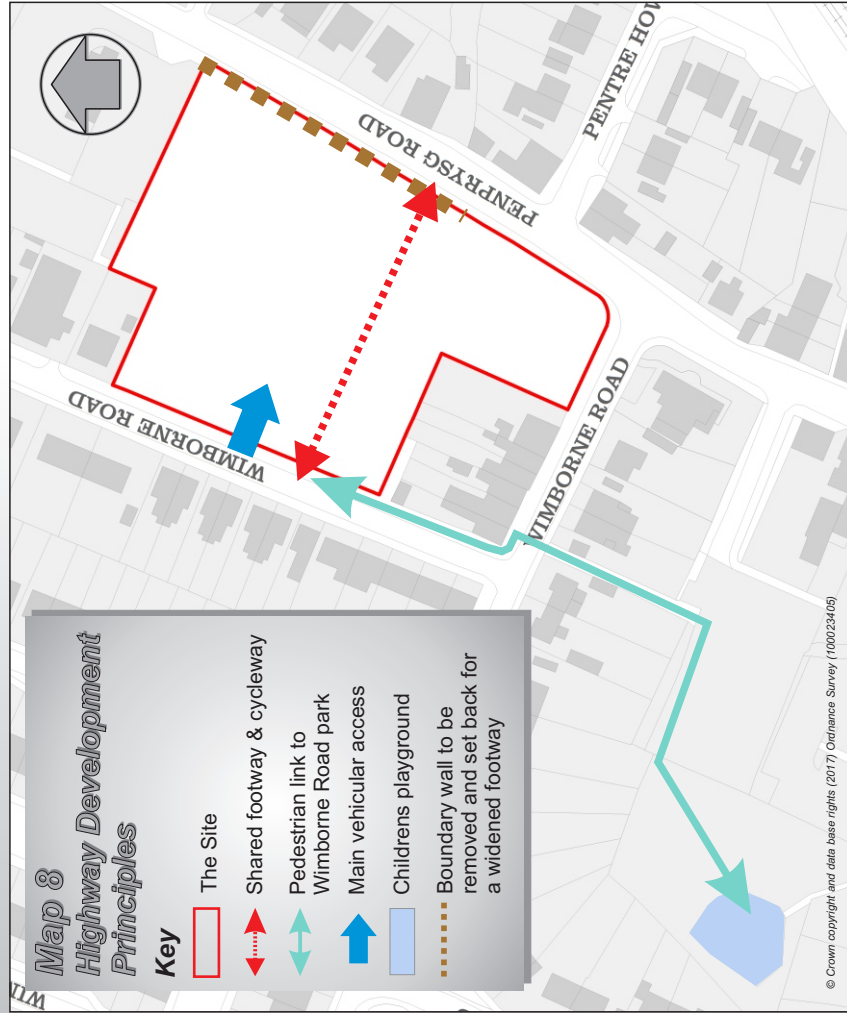
The Active Travel (Wales) Act 2013 aims to make walking and cycling the most attractive option for shorter journeys, in particular, everyday journeys such as to and from a workplace or education establishment or in order to access health, leisure or other services or facilities. The key underlying objectives informing the movement and access strategy for the site are as follows:

- Prioritise pedestrian movement across the site area, especially for individuals with restricted mobility; and
- Promote and improve pedestrian links between the site and the surrounding area.

The above objectives need to be considered in the context of other priorities in this Brief, such as securing regeneration of the area and enhancing the public realm. There are a number of highway considerations that developers should take into account when preparing a development scheme for the site. Any proposal should provide details of:

- Potential effects on the surrounding highway network and traffic management through the provision of a transport assessment;
- Access/egress points;
- Visibility splays;
- Servicing arrangements (and vehicle turning areas);
- Operational parking arrangements;
- Proximity/lease of access to public transport; and
- Opportunities to improve routes and secure parking for cyclists.

Map 8 shows the specific highway development principles for the site that will need to be incorporated into a scheme. Given the size and location of the site, a Manual for Streets approach to highway design is considered appropriate. Car parking should be provided in accordance with the Council's standards set out in SPG 17 - Parking Standards but must not dominate the appearance of the development proposals.



# Design Principles

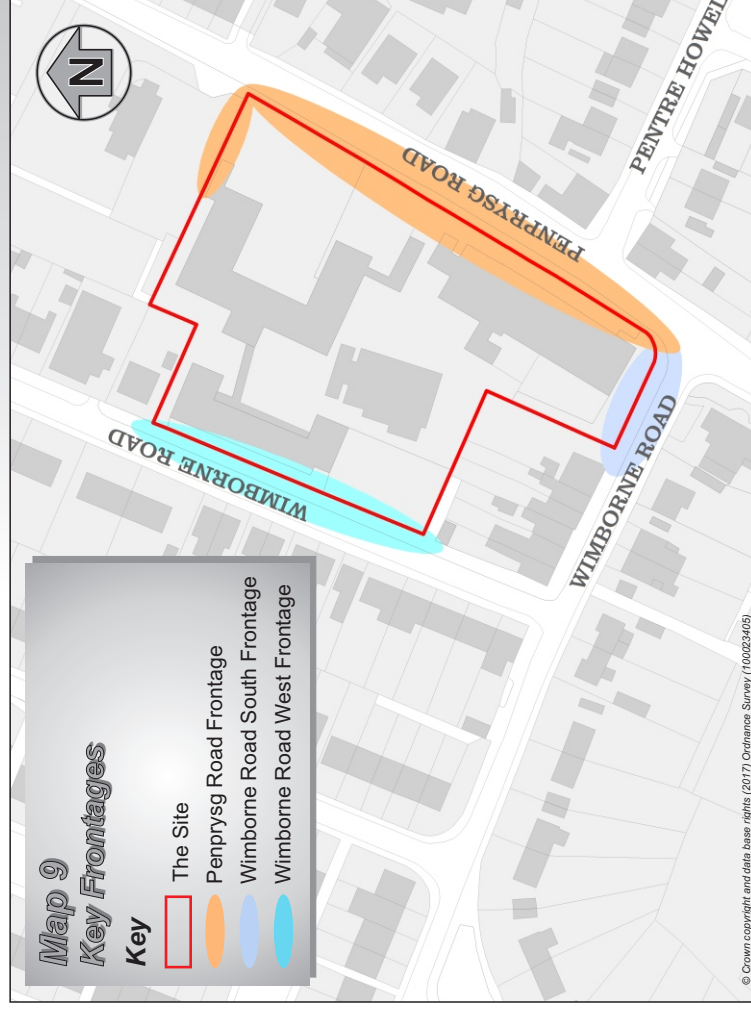
## Key frontages

Map 9 opposite shows the site's key frontages. The site has three frontages and all are considered important and should make a positive contribution to the street scene. This can be achieved by delivering 'active' frontages that are well-designed by adding interest, life and vitality to the public realm. This can be achieved by the articulation of facades in a way which includes: frequent doors and windows with few blank walls; articulation of facades with projections such as bays and porches.

The corner of Wimborne Road and Penprysg Road is visually prominent with two frontages. Buildings on corners need to face two ways; however, many standard building types used by housing developers are rarely able to do this. A tailored design will be required. Corners are best emphasised by incorporating prominent entrances and/or windows at the apex, expressing the height by, for instance, using a 'mansion block' of apartments.

It is essential that there is a clear distinction between public and private space so that all publicly accessible parts of the development are fronted by active frontages - with entrances or at the very minimum, animated frontages where there is visual connectivity between occupants inside the building and those outside in the public realm. Private areas must be inaccessible to the public.

The layout should not result in ambiguous space that is not clearly public or private (such as alleyways or other 'leftover space'). All public open space / realm and streets must be overlooked by development. In this regard care must be taken to avoid a layout and building footprint that result in blank elevations that front a public street.



# Design Principles



## Height, scale and massing

All proposals will be required to provide full justification for the height, scale and massing of any new building through submissions in the 'Design and Access Statement'.

Scale, height and massing of the proposed development on the site will largely be governed by the surrounding buildings; the topography; the general patterns of heights in the area and key views. The Brief envisages that development will be predominantly two storeys fronting Penrysg and Wimborne Road. A three storey element may be considered on the corner with Wimborne Road and Penrysg Road. However, the final layout & design will need to respect the existing pattern and character of development in the immediate area. Attention and care will be needed to protect the setting of St David's Church which is Grade II Listed.



## Residential amenity

The development should provide adequate privacy and allow for the 'quiet enjoyment' of residential properties in accordance with the Council's standards set out in **SPG1 - Dwellings and Domestic Scaled Buildings** and the principles of **SPG2 - Householder Development**. The design of individual buildings and the position of doors and windows, together with the position of buildings in relation to one another and appropriate boundary treatments, should be used to ensure that overlooking and overshadowing of private areas is avoided.

# Design Principles

## Detailed design appearance and materials

The Brief does not prescribe a particular architectural approach, but it does require proper attention to “place-making”. There is an opportunity to achieve contemporary design while reflecting the materials, scale and urban grain of the areas historic context (where appropriate). When appraising designs, consideration should be given to the principles set out in **Planning Policy Wales and TAN12: Design (2016)**.

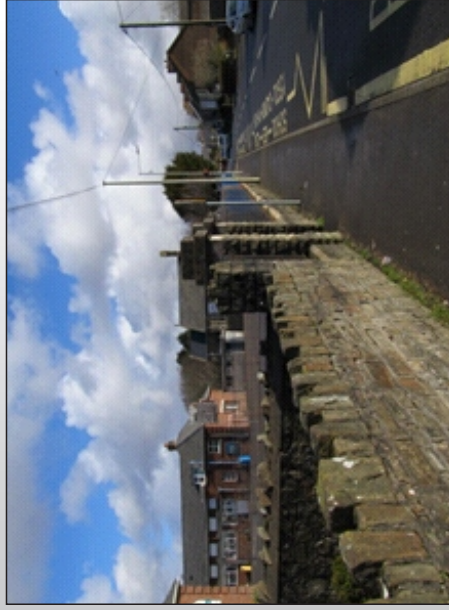
If an option to retain the original school building is proposed, then greater design reference to the existing building should be incorporated into any design. In the event that the developer proposes to demolish the original school building, the reuse of materials should be considered.

Details on the key architectural features of the existing area that need to be respected and reinforced by the development proposal are set out below:

- Maximum of 2 storey development (although 3 storeys may be appropriate on the corner of Wimborne and Penprysg Road);
- Pitched Roofs;
- Key views of St David's Church to be retained (see plan); and
- Energy efficiency measures need to be appropriately incorporated into any design.



# Design Principles



## Public realm and landscape

An overall landscape and planting strategy will need to be prepared which seeks to maintain the quality and future viability of the landscape setting on the site. Development proposals should respond to the site's context and the important setting of St David's Church. Appropriate boundary treatments and landscaping are considered important in this context. Open space should be provided in accordance with Policy COM 11 of the LDP.

### Boundary Treatments:

- Retention of the boundary stone wall should be considered (where appropriate) along the northern boundary of the site; and
- Internal boundary treatments can be more varied including stone walls, railings on walls, railings and planting. Reuse of any materials should be considered.



*Tree and shrub planting provides street-wide enhancement as well as adding value to individual properties*

### Boundary Planting:

- Development proposals should seek to incorporate additional tree planting to improve the arboriculture character of the site. Wherever possible, trees should be native and in-keeping with the surrounding existing arboriculture character. Planting at key junctions/crossing points that are likely to have a negative impact on vision splays, need to have careful consideration to ensure that there is no future detrimental impact on visibility to Pedestrians, particularly children, moving to and from the site.

## Energy Efficiency and Generation

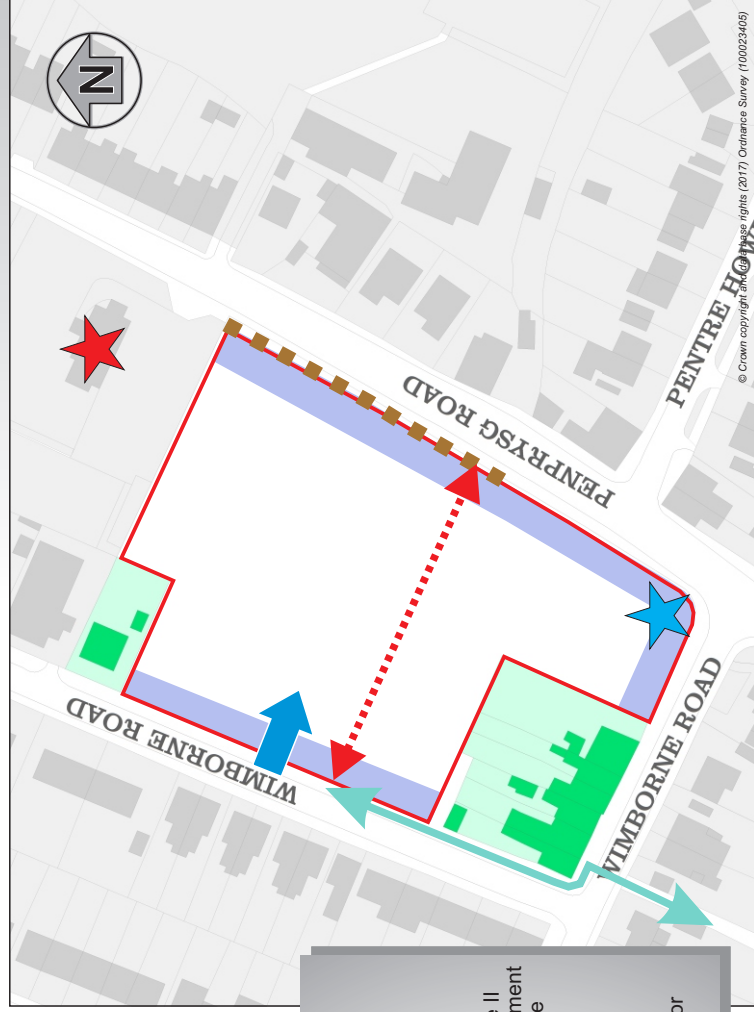
The redevelopment of this site will constitute major development and development proposals will therefore be required to be accompanied by an Energy Assessment as stipulated in Policy ENV17 of the LDP.

## Parameter Plan

In light of the site characteristics, location and policy context, there are considered to be two broad re-development options at the site, namely:









- 1 Retention of the original school building, demolition of modern additions and new residential development; and
- 2 Demolition of the original school building, modern additions and replacement with new residential development.

The parameter plan sets out the key development parameters that must be acknowledged when formulating imaginative design solutions which will maximise the potential of this development site.



**Map 10  
 Parameter Plan**

**Key**

	The Site		Main vehicular access
	Key frontage		St David's Church Grade II Listed Building (development must not compromise the setting of the building)
	Residential amenity protection zone		Opportunity for 3 storey development
	Shared footpath & cycle way		Boundary wall to be removed and set back for a widened footway
	Pedestrian link to Wimborne Road park		

# Planning Application Requirements

## Planning Application Requirements

This section of the Development Planning Design Brief sets out the application documentation that will be required to support any future planning application at the site.

### Application(s) Documentation

A full planning application at the site will need to be supported by:

- Application Forms and Certificates;
- Site Location Plan;
- Statutory Application Fee;
- Plans and Drawings (sufficient for a full planning application including block plans and elevations of existing and proposed development);
- Design and Access Statement – this should include an appraisal which demonstrates how the proposed design responds to the key characteristics of the area;
- Planning Statement including Heads of Terms for a S106 Agreement;
- In the event of a proposal that does not retain the original school building a Heritage Statement demonstrating how the redevelopment proposals will respect and protect the setting of the Grade II Listed Building (St David's Church);
- Ecology Survey and Tree Report – to demonstrate how any ecological impacts will be appropriately mitigated;
- Transport Statement – due to the change in the nature of the use a Transport Statement will be required to demonstrate that there are no unacceptable impacts on the surrounding road network and that the site can be adequately accessed; and
- Surface and Foul Water Drainage Strategy - to demonstrate that the redevelopment of the site will have no unacceptable impacts on foul and surface water drainage.

## Planning Obligations

Whilst the precise planning obligations package will need to be confirmed at the application stage, this section sets out the anticipated planning obligations that would be expected to be secured pursuant to the future redevelopment of the site. The following obligations have been identified following discussions with the Council's Section 106 officer.

### Affordable Housing Provision

The adopted LDP confirms a requirement for 20% provision of affordable housing at the site, given its location within the 'Bridgend, Pencoed & Hinterland' Housing Market Area, as defined by Policy COM5 (e.g. a scheme proposing 30 residential units would need to provide 6 affordable units). The appropriate number of units should be provided on site and transferred to a RSL. The size, type, tenure and transfer price will be subject to assessment at the planning application stage. It should be assumed that Social Housing Grant will not be available.

### Education Contributions

The formula for calculating the number of school age children generated per dwelling (and the financial contribution required) from residential schemes is set out in SPG16: Educational Facilities & Residential Development.

- The site lies within the catchment of Pencoed Primary and Pencoed Comprehensive schools.
- At the time of preparing this brief, Pencoed Comprehensive School had sufficient capacity to accommodate the likely number of school age children generated by a development site of the size envisaged. However, there are capacity issues with all primary schools in the area and therefore development proposals are likely to trigger the need for a contribution towards primary education facilities.

In light of this, and to account for future changes in capacity, any s106 agreement signed in connection with the development of the site would need to include the formula for calculating education contributions.



## Highways Contributions

Highway and Transportation requirements have been highlighted in their relevant section. The precise nature of transport-related contributions, either physical improvements or financial payments, will be determined by the outcome of technical studies to assess the impact of a final detailed scheme on the highway network which will highlight the nature of mitigation works deemed necessary to make the proposals acceptable in planning terms.

## Public Open Space

Taking account of existing levels of provision, all new housing developments are required to satisfy the outdoor recreation space standards of the LDP. The Council's 'Open Space Audit' identifies that there is currently a deficit of equipped children's play equipment and outdoor sports facilities in this location and the following amount of open space provision is required to ensure compliance with Policy COM11:

- A development of 30 dwellings would lead to an estimated development population of 70 (based on an average 2.34 people per house);
- The total amount of outdoor recreation space required is therefore 1,680sqm; and
- This should consist of approximately 1120sqm of Outdoor Sport and 560sqm of Children's Play Space.

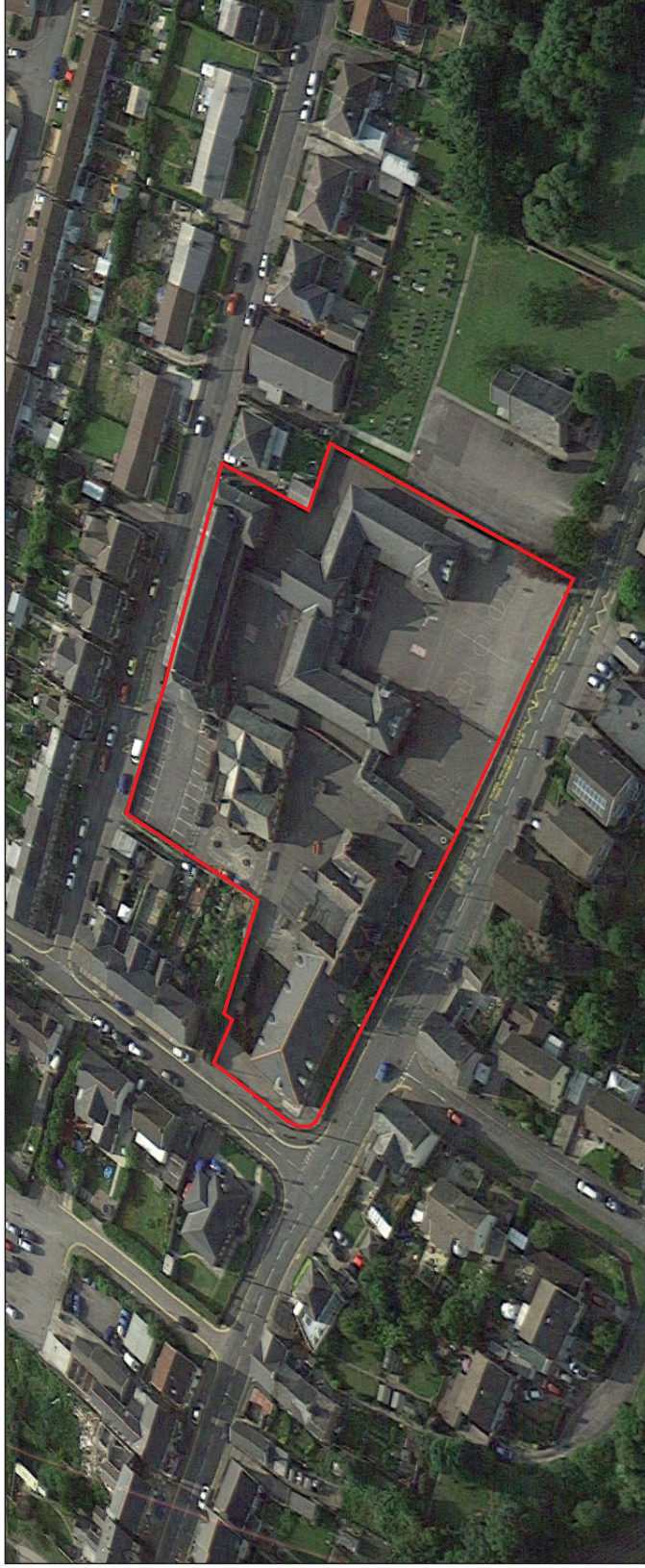
Discussions with the Council have confirmed a preference for an off-site contribution in lieu of onsite provision of outdoor recreation space, due to the scale of the development proposed. This should be provided as follows:

- A financial contribution towards children's play equipment at the Wimborne Road Play Area; and
- A financial contribution towards the provision of off-site sports facilities.

If a developer is minded to provide on-site recreational provision, the LPA do not wish to take on the long term maintenance of the open space and the developer will need to retain ownership and make provision for maintenance through a dedicated management company or use of a third party.

## Conclusion

The site is allocated for residential development in the Local Development Plan and is considered suitable for approximately 20-30 units subject to meeting the requirements of this brief.



# Notes

# Pencoed Primary School Planning Development Brief



Development Planning, Bridgend County Borough Council, Angel Street, Bridgend, CF31 4WB

Tel: 01656 643162, Email: [developmentplanning@bridgend.gov.uk](mailto:developmentplanning@bridgend.gov.uk)